



**City of Bellevue
Development Services Department
Land Use Staff Report**

Proposal Name: 13120 NE Bel-Red Rd

Proposal Address: 13120 NE Bel-Red Rd

Proposal Description: Land Use review of an Administrative Conditional Use Permit to allow an auto retail use and associated improvements in an existing 9,461 square-foot building previously use for retail.

File Number: 19-128067-LA

Applicant: Lance Mueller, Lance Mueller and Associates

Decisions Included: Administrative Conditional Use Permit
(Process II 20.30E)

Planner: Reilly Pittman, Planner

**State Environmental Policy Act
Threshold Determination:** **Exempt**

Director's Decision: **Approval with Conditions**
Heidi M. Bedwell,
By: *Environmental Planning Manager* for
Michael A. Brennan, Director
Development Services Department

Application Date: October 25, 2019
Notice of Application Publication Date: November 21, 2019
Decision Publication Date: April 9, 2020
Appeal Deadline: April 23, 2020

For information on how to appeal a proposal, visit Development Services Center at City Hall or call (425) 452-6800. Appeal of the Decision must be received in the City's Clerk's Office by 5 PM on the date noted for appeal of the decision.

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Documents in File

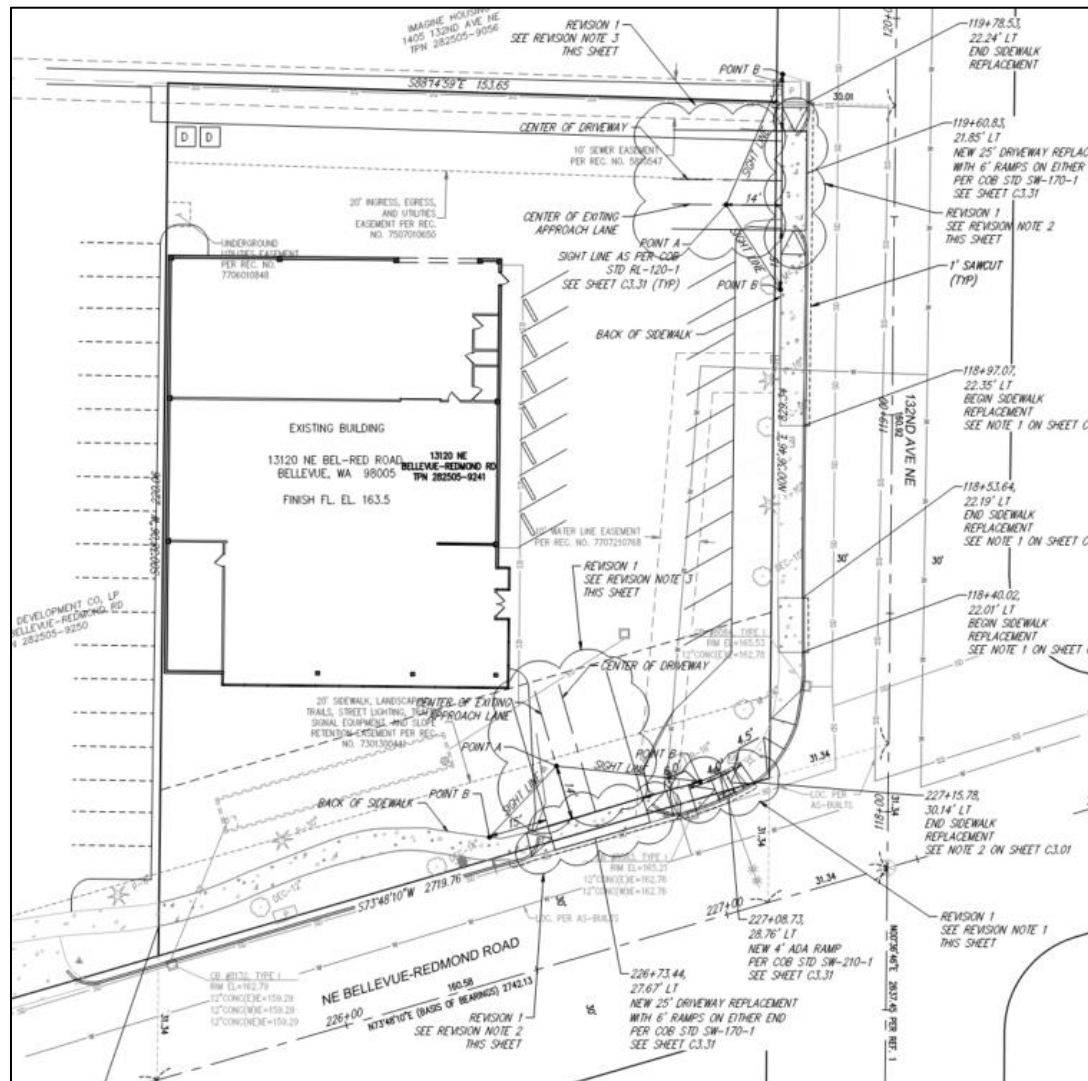
1. Project Plans – Attached
2. Communication – In File
3. Application forms – In File

I. Request and Review Process

A. Request

The applicant proposes to convert an existing 9,461 square-foot building used for retail to an auto retail use. No other site development or changes are proposed under this application. See figure 1 below for existing site.

Figure 1



B. Review Process

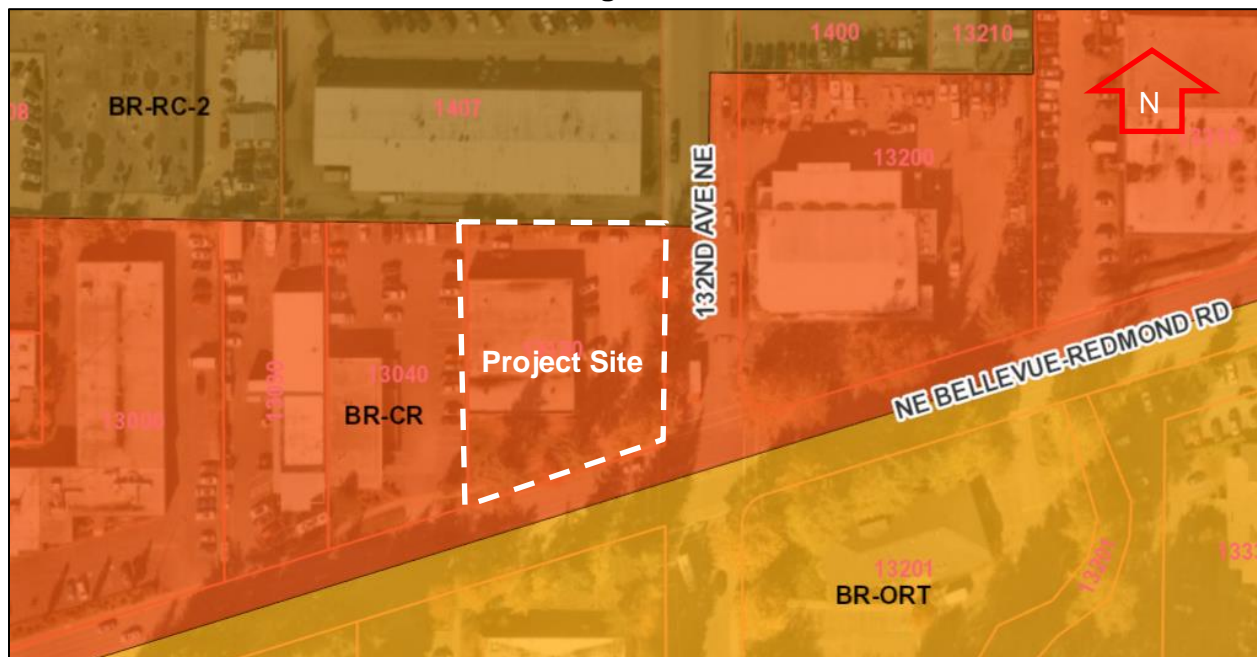
Land Use Code 20.25D.070 states which uses are permitted in BelRed zoning districts of the City of Bellevue. Auto Retail is allowed in the BR-CR, BelRed Commercial Residential zone only when proposals are consistent with the decision criteria for an Administrative Conditional Use Permit (ACUP) in LUC 20.30E.140. An ACUP is a Process II administrative decision issued by the Director of Development Services and appealable to a hearing examiner.

II. Site Context and Description

A. Site Context

The site is located in the Bel-Red subarea of the City on a property that is zone BR-CR, BelRed Commercial Residential. The property is a corner lot that has frontage on Bellevue Redmond Rd. to the south and 132nd Ave NE along the east property line. The property has vehicle access onto both roads. Properties to the north are zoned BR-CR-2, BelRed Commercial Residential Node 2. Properties to the east and west are zoned BR-CR. Properties to the south, across Bel-Red Rd. are zoned BR-ORT, BelRed Office Residential Transition. All uses in vicinity are commercial uses. **See Figure 1 below for a zoning map.**

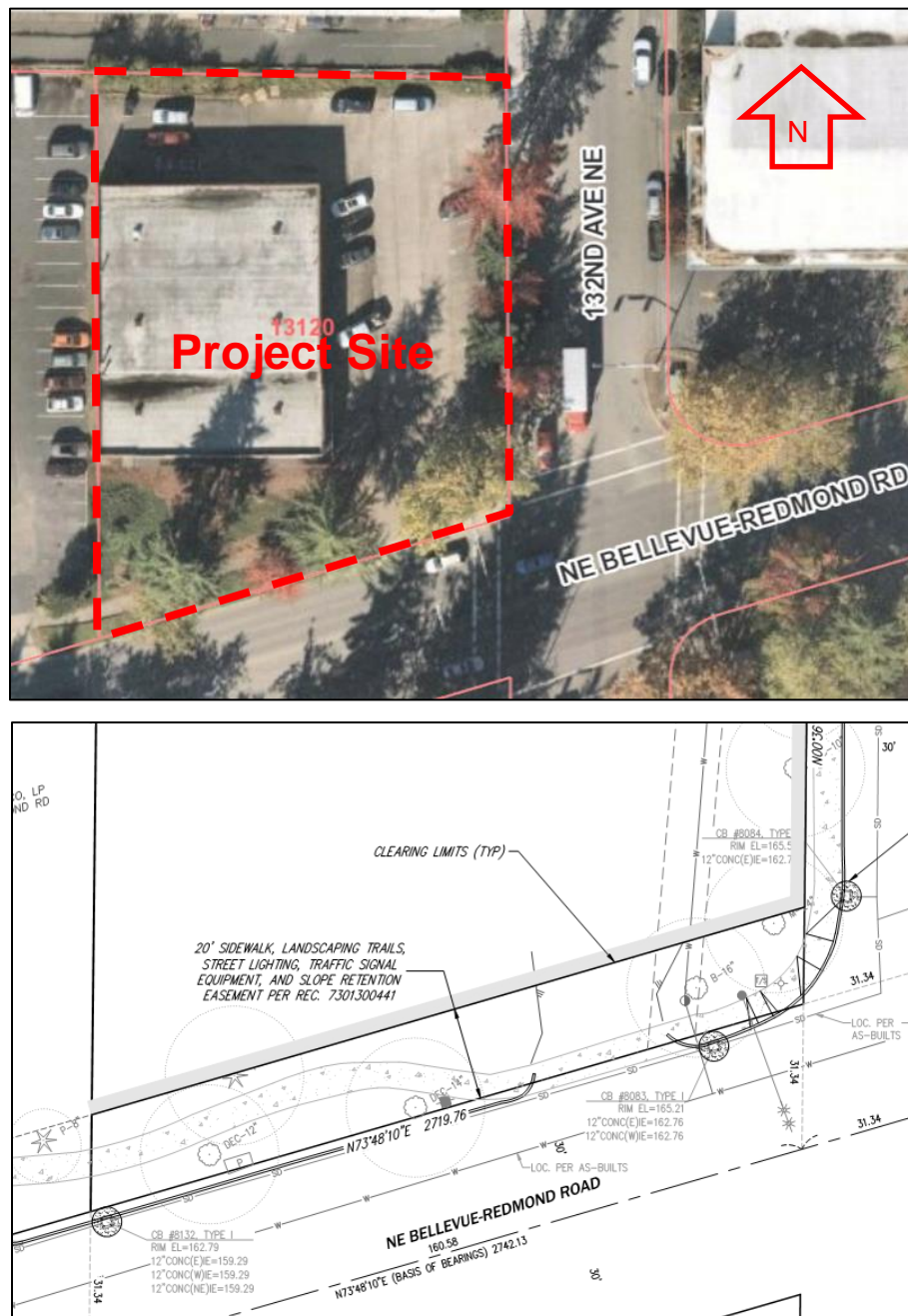
Figure 1



B. Site Description

The site has one building that historically housed general retail uses. The building is setback from both street frontages and is very close to the western property line. There is parking found to the north and east of the building. To the south, the site is landscaped between the building and Bel-Red Rd. The sidewalk along Bel-Red Rd crosses the site separated from the road, within a public easement. The significant trees along the Bel-Red Rd. frontage and sidewalk are public street trees maintained by the Parks Department. **See figure 2 below for general existing site conditions and sidewalk easement.**

Figure 2



III. Consistency with Development Requirements

A. Conformance with LUC 20.25D.080 and LUC 20.25D.100 – Dimensional Requirements and Automobile Sales

The proposal does not propose to alter the exterior footprint of the existing structure. Conformance with the zoning dimensional requirements for the BR-CR zone is not

triggered. Auto retail uses are required to meet applicable requirements of LUC 20.25D.100 below.

1. The applicant shall provide adequate off-street unloading area for vehicle carriers

The submitted truck turning radius shows that the site can be accessed by smaller vehicle carrier trucks. A full size vehicle carrier cannot access the site. The Land Use Code does not specify the size of vehicle carrier allowed and the applicant is proposing to sell autos with a high price point that will limit on-site stock. However, approval of an ACUP approves the site for auto retail sales and would allow a future tenant who needs higher stock on-site. An auto sales use is approved on this site with the condition that only smaller trucks are used to load and unload cars on the site. There is no unloading of cars in the City's right-of-way allowed under this ACUP. Violation of this condition is grounds for revocation of the ACUP approval per LUC 20.30E.170.B. **See Conditions of Approval in Section X for condition on vehicle unloading.**

2. Limitations on Outside Storage and Displays

b. Properties abutting the north side of Bellevue-Redmond Road. No outside storage or display is permitted between the building and Bellevue-Redmond Road

No external display or other redevelopment is proposed as part of this ACUP application. Future improvement of the property will require conformance with this code. **See Conditions of Approval in Section X for condition on future development and improvement of the property.**

B. Conformance with LUC 20.25D.120 – Parking, Circulation, and Internal Walkway Requirements

The proposed auto retail use is an unspecified use in the parking code for Bel-Red zones. The applicant has provided information regarding proposed employees on the site and the nature of the use being a low volume seller of high priced cars. The applicant has shown the existing parking on the site is sufficient for the proposed use. Future redevelopment or improvement of the site may require proportional compliance under LUC 20.25D.060, to improve nonconforming site improvements such as parking and landscaping. **See Conditions of Approval in Section X for condition on proportional compliance.**

C. Conformance with LUC 20.25D.140.D – BelRed Street Development Standards, Green Streets

The northern property line of the site abuts a planned green street found on the map in LUC 20.25D.140. Future redevelopment or substantial improvement of the site will require dedication for this planned green street. **See Conditions of Approval in Section X for condition on future development triggering street dedication.**

IV. Public Notice and Comment

Application Date:	October 25, 2019
Public Notice (500 feet):	November 21, 2019
Minimum Comment Period:	December 5, 2019

The Notice of Application for this project was published in the City of Bellevue weekly permit bulletin and Seattle Times on November 21, 2019. It was mailed to property owners within 500 feet of the project site and a public notice sign was posted. No comments were submitted.

V. Summary of Technical Reviews

A. Clearing and Grading

The Clearing and Grading Division reviewed the proposed use and approved the application.

B. Utilities

The Utilities Department reviewed the proposed use and approved the application.

C. Transportation

The Bel-Red Auto Sales project will result in a net reduction of six trips when compared to the existing use, which is below the 30-trip threshold that requires concurrency analysis. Therefore, a concurrency analysis is not required.

i. Site Access and Loading

The Bel-Red Auto Sales project is located at the northwest corner of the intersection of NE Bel-Red Road and 132nd Avenue NE. The site has an existing 9,641 square foot building that is currently is used as Retail space. The site has access from Bel-Red Road via an existing 29-foot-wide full access driveway, and from 132nd Avenue NE via an existing 22-foot-wide full access driveway. Bel-Red Road is a five lane road classified as a major arterial, and 132nd Avenue NE is a three lane road classified as a collector arterial. There is currently a 6-foot-wide sidewalk along the 132nd Avenue NE frontage and a 6-foot-wide sidewalk with a varying planter strip along the Bel-Red Road frontage.

Access to the proposed project will be provided via a reconstructed 25-foot-wide driveway on NE Bel-Red Road and a reconstructed 25-foot-wide driveway on 132nd Avenue NE. The driveways will not have any turning restrictions with this development. In the future, the City reserves the right to restrict left turns into and out of this site if this driveway is determined to be an operational or safety hazard.

Pedestrian access to the site will be provided by the existing 6-foot-wide sidewalk along Bel-Red Road and 132nd Avenue NE. Any sidewalk panels along the frontage

that are heaved or damaged will be replaced to maintain ADA compliant pedestrian access to the site. The curb ramps and pedestrian pushbuttons will also be upgraded to meet current ADA standards on the north side of Bel-Red Road.

All garbage pickup, vehicle loading, and car carrier loading is required to occur on-site. Truck movements were verified with Autoturn diagrams for a WB-40 truck accessing the site. Loading on Bel-Red Road and 132nd Avenue NE will not be allowed. **See Conditions of Approval in Section X for condition on access restriction and loading.**

ii. Street Frontage Improvements

The Bel-Red Auto Sales project is located at the northwest corner of the intersection of NE Bel-Red Road and 132nd Avenue NE. There is currently a 6-foot-wide sidewalk with curb and gutter along the 132nd Avenue NE frontage and a 6-foot-wide sidewalk, a varying width planter strip, and curb and gutter along the Bel-Red Road frontage. Some of the existing sidewalk has damage or has been heaved by tree roots. There are pedestrian curb ramps at the southeast corner of the parcel to cross Bel-Red Road to the south and 132nd Avenue NE to the east. The curb ramp on the north side of Bel-Red Road is currently out of compliance with current ADA regulations as are the driveway approaches on NE Bel-Red Road and 132nd Avenue NE. The existing ramp on 132nd Avenue NE appears to meet current ADA standards but will be required to be replaced if it is shown to be substandard.

In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval. The design of the improvements must conform to the requirements of the Americans with Disabilities Act, the Transportation Development Code (BCC 14.60), and the provisions of the Transportation Department Design Manual.

Frontage improvements are required and shall include the following:

1. Bel-Red Road:

- Install a minimum 25-foot-wide driveway to replace the existing driveway approach meeting standard detail SW-170-1.
- Replace any curb and gutter along the frontage that is damaged.
- Replace the existing sidewalk panels that are damaged, defective, or heaved.
- Upgrade the existing ADA curb ramp and pedestrian pushbutton on the north side of Bel-Red Road to meet current ADA standards.
- Add or relocate pedestrian pushbuttons to meet current ADA standards.
- Replace any vaults in the sidewalk that do not have non-slip ADA compliant

lids.

- Provide a photometric street lighting analysis showing that the minimum lighting levels are met along the project frontage.

2. 132nd Avenue NE:

- Install a minimum 25-foot-wide driveway to replace the existing driveway approach meeting standard detail SW-170-1.
- Replace any curb and gutter along the frontage that is damaged.
- Replace the existing sidewalk panels that are damaged, defective, or heaved.
- Replace the existing curb ramp if it does not meet current ADA standards,
- Add or relocate pedestrian pushbuttons to meet current ADA standards.
- Provide sidewalk easements for any portion of the frontage improvements that extend onto private property.
- Replace any vaults in the sidewalk that do not have non-slip ADA compliant lids.
- Provide a photometric street lighting analysis showing that the minimum lighting levels are met along the project frontage.

See Conditions of Approval in Section X for condition on street frontage improvements and civil engineering plans.

3. Easements

The applicant shall provide sidewalk and utility easements to the City as needed to encompass the full required width of any located outside the city right of way fronting this site. Any negative impact that this development has on those easements must be mitigated or easements relinquished.

The applicant shall provide easements to the City for location of signal and pedestrian push button facilities consisting of above-grade boxes and/or below-grade vaults between the building and sidewalk within the landscape area on the Bel-Red Road and 132nd Avenue NE frontage. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.

See Conditions of Approval in Section X for condition on existing easements, easements for pedestrian signal equipment and vaults, and sidewalk/utility easements.

4. Use of the Right of Way During Construction

Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and

street improvements. A Right of Way Use Permit for such activities must be acquired prior to issuance of any construction permit including demolition permit. Sidewalks may not be closed except as specifically allowed by a Right of Way Use Permit.

See Conditions of Approval in Section X for condition on right-of-way use permit.

5. Pavement Restoration

The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted," "Overlay Required," and "Standard Trench Restoration." Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching.

Near this project, Bel-Red Road has been classified as Standard Trench Restoration per standard drawing RC-190-1.

Near the development site, 132nd Avenue NE is classified as Grind and Overlay required. Should street cuts prove unavoidable or if the street surface is damaged in the construction process, a half-street or full-street (depending on the extent of street cuts or damage) grind and overlay will be required for a minimum of 50 feet.

See Conditions of Approval in Section X for condition on pavement restoration.

D. Fire Prevention

The Fire Department reviewed the proposed use and approved the application.

Future development of the property will be required to meet any applicable code requirements. Reviewers have reviewed the proposed auto retail use and approved the ACUP but no development of the site is currently proposed.

VI. State Environmental Policy Act (SEPA)

The proposal is exempt from SEPA per WAC 197-11-800(1) as there is no new construction.

VII. Changes to Proposal Due to Staff Review

The applicant was required to address parking required for the proposed use as well as demonstrate unloading of cars and other transportation requirements.

VIII. Decision Criteria

A. 20.30E.140 Administrative Conditional Use Permit - Decision Criteria

The Director may approve, or approve with modifications, an application for an Administrative Conditional Use Permit.

1. The administrative conditional use is consistent with the Comprehensive Plan.

Finding: The project is consistent with the Comprehensive Plan. The proposal is to reuse an existing commercial building in a land use district that allows retail and other commercial uses and is consistent with the Bel-Red Land Use Plan map (S-BR-5). The proposal provides additional retail capacity in an area zoned for retail (LU-18).

2. The design is compatible with and responds to the existed or intended character, appearance, quality of development and physical characteristics of the subject property and immediate vicinity.

Finding: The building footprint will remain unchanged as discussed in this report. If any minor external material changes are proposed they will require a Land Use Exemption from Design Review as part of the building permit. Redevelopment of the site will require a Design Review application. **See Conditions of Approval in Section X for condition on future development.**

3. The administrative conditional use will be served by adequate public facilities including streets, fire protection and utilities.

Finding: The proposal does not change the existing building that is currently served by adequate services. The proposed use as an auto retail use does not increase demand on public facilities. All unloading of cars is required to occur on site.

4. The administrative conditional use will not be materially detrimental to uses or property in the immediate vicinity of the subject property

Finding: The proposal is located in a zone that allows retail uses. Given the intent to occupy the site versus redeveloping it there will not be any change that will impact other uses or property in the vicinity. All unloading of cars will occur onsite and all retail activity will occur within the building. No outside storage or display is permitted between the building and Bellevue-Redmond Road. The model proposed for this use is to have low volume sales that does not have large inventory on site. Cars are intended to be delivered as needed based on purchase.

5. The administrative conditional use complies with the applicable requirements of this code.

Finding: As conditioned, the proposal complies with applicable requirements of the Land Use Code. Refer to section III of this report for further information on Land Use Code consistency.

IX. Conclusion and Decision

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency and City Code & Standard compliance reviews, the Director does hereby **APPROVE WITH CONDITIONS** the subject proposal to site an auto retail use.

X. Conditions of Approval

The applicant shall comply with all applicable Bellevue City Codes and Ordinances including but not limited to:

Applicable Ordinances	Contact Person
Clearing and Grading Code- BCC 23.76	Janney Gwo, 425-452-6190
Utilities – BCC Title 24	Sean Wells, 425-452-4855
Land Use Code- BCC Title 20	Reilly Pittman, 425-452-4350
Noise Control- BCC 9.18	Reilly Pittman, 425-452-4350
Transportation Code – BCC 14.60	Ian Nisbet, 425-452-4851
Right of Way Use Permit – BCC 14.30	Tim Stever, 425-452-4294
Fire Code – BCC 23.11	Glen Albright, 425-452-4270

The following conditions are imposed under the Bellevue City Code authority referenced:

A. General Conditions

1. Building Permit

Approval of this Administrative Conditional Use Permit does not constitute an approval of any construction permit. A building permit for a change of use tenant improvement is required prior to occupancy. Plans submitted as part of any permit application shall be consistent with the activity permitted under this approval.

Authority: Land Use Code 20.30E.140

Reviewer: Reilly Pittman, Development Services Department

2. Future Design Review

No redevelopment is currently proposed on the plans. Any exterior changes proposed in the future may require an application for design review or an exemption from design review

Authority: Land Use Code 20.25D.030

Reviewer: Reilly Pittman, Development Services Department

3. Limited Unloading and Revocation of Approval

An auto sales use is approved on this site with the condition that only trucks reviewed and approved by the Transportation Department as discussed in this report are used to load and unload cars on the site. There is no unloading of cars in the City's right-of-way allowed under this approval. Unload and loading cars in the right-of-way is a violation of this condition is grounds for revocation of the ACUP approval per LUC 20.30E.170.B. Future auto sales uses on this site are subject to this restriction unless the site area is enlarged.

Authority: Land Use Code 20.25D.100

Reviewer: Reilly Pittman, Development Services Department

4. Outside Displays

No outside storage or display is permitted between the building and Bellevue-Redmond Road.

Authority: Land Use Code 20.25D.100

Reviewer: Reilly Pittman, Development Services Department

5. Proportional Compliance

Future redevelopment or improvement of the site requires proportional compliance per LUC 20.25D.060 to address nonconforming site improvements such as parking and landscaping.

Authority: Land Use Code 20.25D.060

Reviewer: Reilly Pittman, Development Services Department

6. Future ROW Dedication

Future redevelopment of this property will be subject to any right-of-way dedication required in LUC 20.25D.140 for BelRed Street Development Standards or as amended.

Authority: Land Use Code 20.25D.140

Reviewer: Reilly Pittman, Development Services Department

7. Provisions for Loading

The property owner shall provide an off-street loading space which can access a public street. This must include an off-street location for garbage pick-up, which must be acceptable to the garbage hauler. Adequate space for car haulers must also be provided on site. On-street loading and unloading will not be permitted.

Authority: LUC 20.20.590.K.4; BCC 14.60.180

Reviewer: Ian Nisbet, Transportation Department

B. Prior to Issuance of any Permit

8. Right-Of-Way Use Permit

Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- a) Designated truck hauling routes.
- b) Truck loading/unloading activities.
- c) Location of construction fences.
- d) Hours of construction and hauling.
- e) Requirements for leasing of right of way or pedestrian easements.
- f) Provisions for street sweeping, excavation and construction.
- g) Location of construction signing and pedestrian detour routes.
- h) All other construction activities as they affect the public street system.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevent access. General materials storage and contractor convenience are not reasons for preventing access.

The applicant shall secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.

Authority: BCC 11.70 & 14.30

Reviewer: Tim Stever, Transportation Department

9. Civil Engineering Plans – Transportation

Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway accesses must be in conformance with the requirements of the Americans with Disabilities Act, the Transportation Development Code, the provisions of the Transportation Department Design Manual, and specific requirements stated elsewhere in this document.

All relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the final engineering plans. Requirements for the engineering plans include, but are not limited to:

- a) Traffic signs and markings.
- b) Curb, gutter, sidewalk, and driveway approach design. The engineering plans shall

be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.

- c) Curb ramps, crosswalk revisions, and crosswalk equipment such as pushbuttons.
- d) Installation or relocation of streetlights and related equipment.
- e) Undergrounding of existing overhead utility lines, which should be coordinated with adjacent sites. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.
- f) Sight distance. Show the required sight triangles and include any sight obstructions, including those off-site. Sight distance triangles must be shown at all driveway locations and must consider all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.
- g) Landings on sloping approaches are not to exceed a 7 % slope for a distance of 30 feet approaching the back edge of sidewalk. Driveway grade must be designed to prevent vehicles from bottoming out due to abrupt changes in grade.
- h) Location of fixed objects in the sidewalk or near the driveway approach.
- i) Trench restoration within any right of way or access easement.

Frontage improvements are required and shall include the following:

1. Bel-Red Road:

- Install a minimum 25-foot-wide driveway to replace the existing driveway approach meeting standard detail SW-170-1.
- Replace any curb and gutter along the frontage that is damaged.
- Replace the existing sidewalk panels that are damaged, defective, or heaved.
- Upgrade the existing ADA curb ramp and pedestrian pushbutton on the north side of Bel-Red Road to meet current ADA standards.
- Add or relocate pedestrian pushbuttons to meet current ADA standards.
- Replace any vaults in the sidewalk that do not have non-slip ADA compliant lids.
- Provide a photometric street lighting analysis showing that the minimum lighting levels are met along the project frontage.

2. 132nd Avenue NE:

- Install a minimum 25-foot-wide driveway to replace the existing driveway approach meeting standard detail SW-170-1.
- Replace any curb and gutter along the frontage that is damaged.
- Replace the existing sidewalk panels that are damaged, defective, or heaved.
- Replace the existing curb ramp if it does not meet current ADA standards.
- Add or relocate pedestrian pushbuttons to meet current ADA standards.
- Provide sidewalk easements for any portion of the frontage improvements that extend onto private property.

- Replace any vaults in the sidewalk that do not have non-slip ADA compliant lids.
- Provide a photometric street lighting analysis showing that the minimum lighting levels are met along the project frontage.

Authority: BCC 14.60; Transportation Department Design Manual; Americans with Disabilities Act

Reviewer: Ian Nisbet, Transportation Department

C. Prior to Issuance of any Building Permit

10. Existing Easements

Any utility easements contained on this site which are affected by this development must be identified. Any negative impact that this development has on those easements must be mitigated or easements relinquished.

Authority: BCC 14.60.100

Reviewer: Tim Stever, Transportation Department

11. Easements for Pedestrian Signal Equipment and Street Light Boxes and Vaults

The applicant shall provide easements to the City for location of pedestrian pushbutton signal equipment and street light facilities such as above-grade boxes and below-grade vaults between the building and sidewalk and/or within the landscape area.

Authority: BCC 14.60.100

Reviewer: Ian Nisbet, Transportation Department

12. Sidewalk/Utility Easements

The applicant shall provide sidewalk and utility easements to the City such that sidewalks outside of the City right of way along the property frontage are located within a pedestrian easement area.

Authority: BCC 14.60.100

Reviewer: Ian Nisbet, Transportation Department

D. Prior to Issuance of any Certificate of Occupancy

13. Street Frontage Improvements

All street frontage improvements and other required transportation elements, including streetlight and traffic signal revisions, must be constructed by the applicant and accepted by the Transportation Department inspector. All existing streetlight and traffic signal apparatus affected by this development, including traffic controllers, pedestrian signal poles, traffic signal poles, and power sources, must be relocated as necessary.

Existing overhead lines must be relocated underground. All required improvements must be constructed as per the approved plans or as per direction of the Transportation Department inspector. Bonding or other types of assurance devices will not be accepted in lieu of construction, unless the City requires a delay.

Authority: BCC 14.60; Comprehensive Plan Policy UT-39; Transportation Department Design Manual and Transportation Department Design Manual Standard Drawings.

Reviewer: Ian Nisbet, Transportation Department

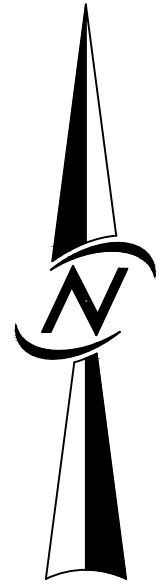
14. Pavement Restoration

Pavement restoration associated with street frontage improvements or to repair damaged street surfaces shall be provided as follows:

- a) Bel-Red Road has been classified as Standard Trench Restoration per standard drawing RC-190-1.
- b) 132nd Avenue NE is classified as Grind and Overlay required. Should street cuts prove unavoidable or if the street surface is damaged in the construction process, a half-street or full-street (depending on the extent of street cuts or damage) grind and overlay will be required for a minimum of 50 feet.

Authority: BCC 14.60. 250; Design Manual Design Standard #23

Reviewer: Tim Stever, Transportation Department



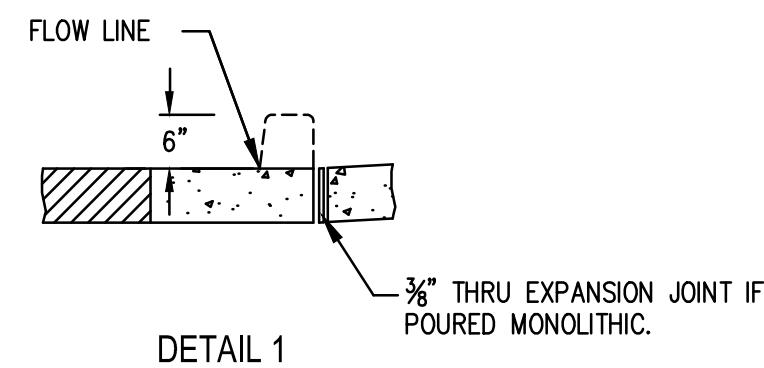
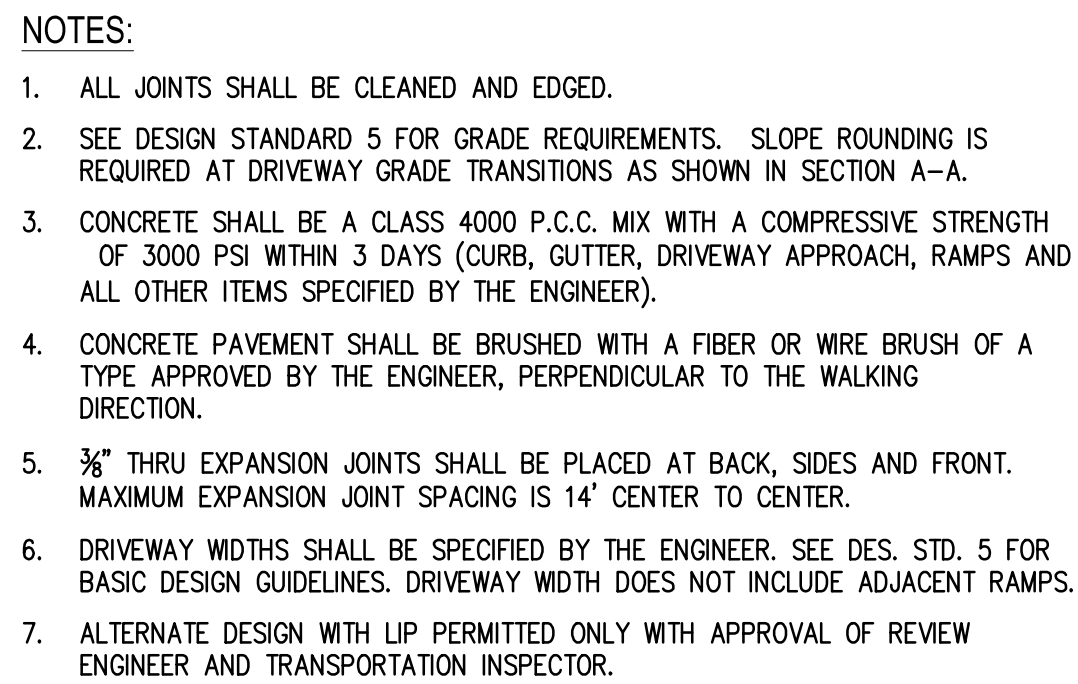
- ALL CLEARING & GRADING CONSTRUCTION MUST BE IN ACCORDANCE WITH CITY OF BELLEVUE (COB) CLEARING & GRADING CODE, CLEARING & GRADING DEVELOPMENT STANDARDS, LAND USE CODE, UNIFORM BUILDING CODE, PERMIT CONDITIONS, AND ALL OTHER APPLICABLE CODES, ORDINANCES, AND STANDARDS. THE DESIGN ELEMENTS WITHIN THESE PLANS HAVE BEEN REVIEWED ACCORDING TO THESE REQUIREMENTS. ANY VARIANCE FROM ADOPTED EROSION STANDARDS IS NOT ALLOWED UNLESS SPECIFICALLY APPROVED BY THE CITY OF BELLEVUE DEVELOPMENT SERVICES (DSD) PRIOR TO CONSTRUCTION. IT SHALL BE THE SOLE RESPONSIBILITY OF THE APPLICANT AND THE PROFESSIONAL CIVIL ENGINEER TO CORRECT ANY ERROR, OMISSION, OR VARIATION FROM THE ABOVE REQUIREMENTS FOUND IN THESE PLANS. ALL CORRECTIONS SHALL BE AT NO ADDITIONAL COST OR LIABILITY TO THE COB.
2. APPROVAL OF THIS EROSION/SEDIMENTATION CONTROL (ESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G. SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).
3. A COPY OF THE APPROVED PLANS AND DRAWINGS MUST BE ON-SITE DURING CONSTRUCTION. THE APPLICANT IS RESPONSIBLE FOR OBTAINING ANY OTHER REQUIRED OR RELATED PERMITS PRIOR TO BEGINNING CONSTRUCTION.
4. THE IMPLEMENTATION OF THESE ESC PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED AND VEGETATION/LANDSCAPING IS ESTABLISHED.
5. THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT-LADEN WATER DO NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
6. THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT-LADEN WATER DO NOT LEAVE THE SITE.
7. ALL LOCATIONS OF EXISTING UTILITIES HAVE BEEN ESTABLISHED BY FIELD SURVEY OR OBTAINED FROM AVAILABLE RECORDS AND SHOULD, THEREFORE, BE CONSIDERED ONLY APPROXIMATE AND NOT NECESSARILY COMPLETE. IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INDEPENDENTLY VERIFY THE ACCURACY OF ALL UTILITY LOCATIONS AND TO DISCOVER AND AVOID ANY OTHER UTILITIES NOT SHOWN WHICH MAY BE AFFECTED BY THE IMPLEMENTATION OF THIS PLAN.
8. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED IN THE FIELD PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE FLAGGED CLEARING LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE APPLICANT CONTRACTOR FOR THE DURATION OF CONSTRUCTION.
9. CLEARING SHALL BE LIMITED TO THE AREAS WITHIN THE APPROVED DISTURBANCE LIMITS. EXPOSED SOILS MUST BE COVERED AT THE END OF EACH WORKING DAY WHEN WORKING FROM OCTOBER 1ST THROUGH APRIL 30TH. FROM MAY 1ST THROUGH SEPTEMBER 30TH, EXPOSED SOILS MUST BE COVERED AT THE END OF EACH CONSTRUCTION WEEK AND ALSO AT THE THREAT OF RAIN.
10. AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A TRAPPED CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM SYSTEM.
11. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT.
12. THE CONTRACTOR MUST MAINTAIN A SWEEPER ON SITE DURING EARTHWORK AND IMMEDIATELY REMOVE SOIL THAT HAS BEEN TRACKED ON PAVED AREAS AS RESULT OF CONSTRUCTION.
13. THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.

1. CONTRACTOR TO LOCATE EDGE OF SIDEWALK PANEL CLOSEST TO MARKED STATION.
2. UTILITIES AND EXISTING STRUCTURES TO REMAIN DURING CONSTRUCTION.

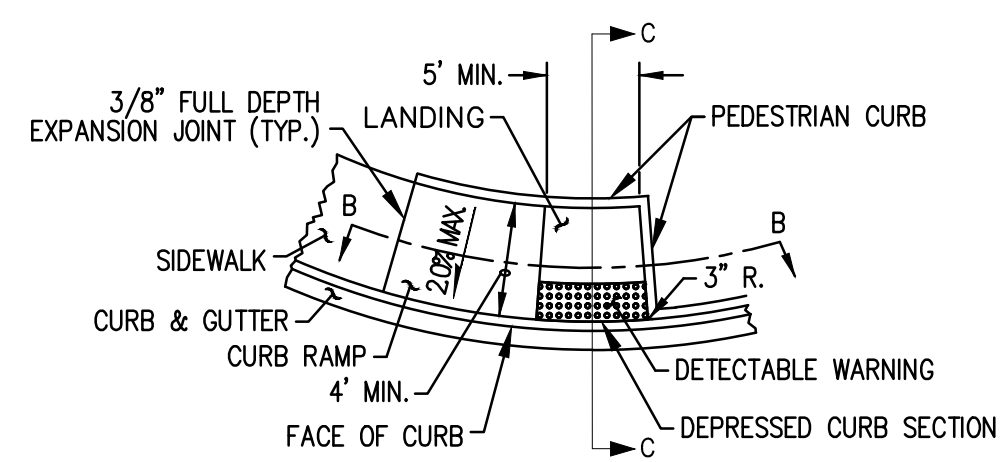
1. CHANGED THE RAMP SHOWN ON THE SITE PLAN TO MATCH THE GIVEN RAMP DETAIL IN SHEET C3.31.
2. CHANGED THE RAMP WINGS ON THE DRIVEWAYS FROM TRIANGULAR TO RECTANGULAR TO MATCH THE DRIVEWAY DETAIL SPECIFIED IN SHEET C3.31.
3. ADDED PEDESTRIAN SIGHT LINES TO DRIVEWAY ON THE SITE PLAN AND ADDED CORRESPONDING DETAIL TO SHEET C3.31.

PERMIT NO. 20-XXXXXX GD
C.O.B. GRID #X & #X

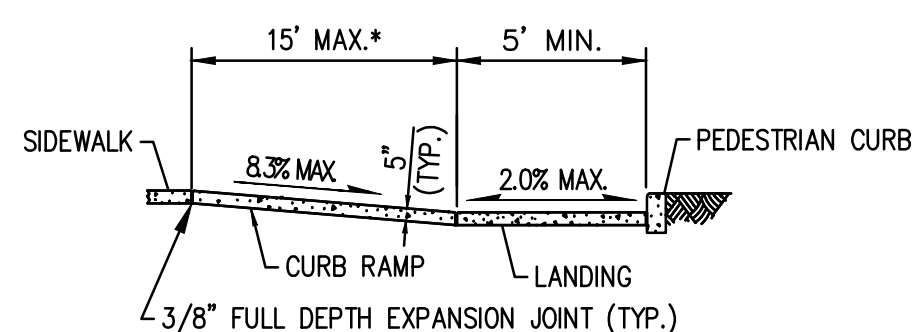
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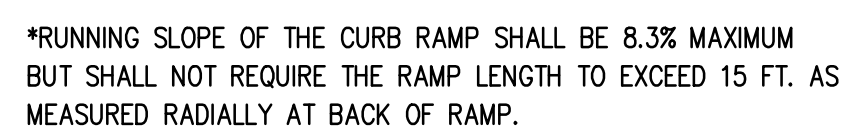
COB STD SW-170-1
SCALE: NONE



CURB RAMP TYPE PARALLEL B



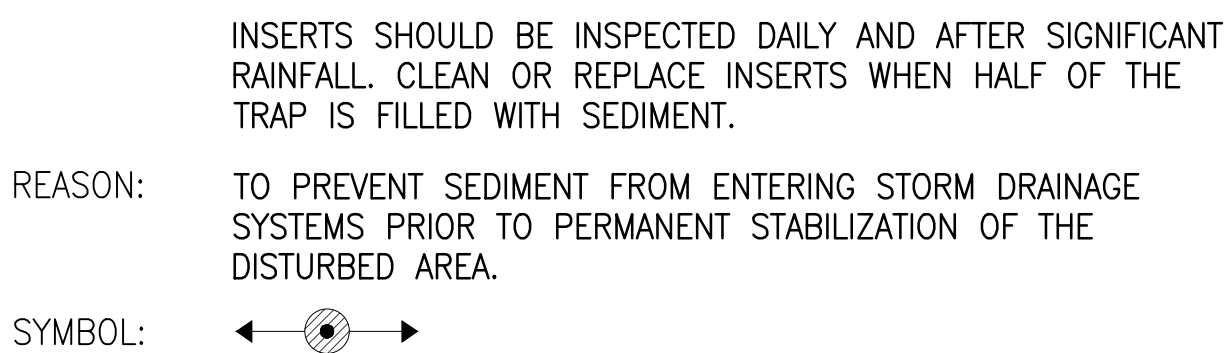
SECTION B-B



LEGEND

↔ SLOPE IN EITHER DIRECTION

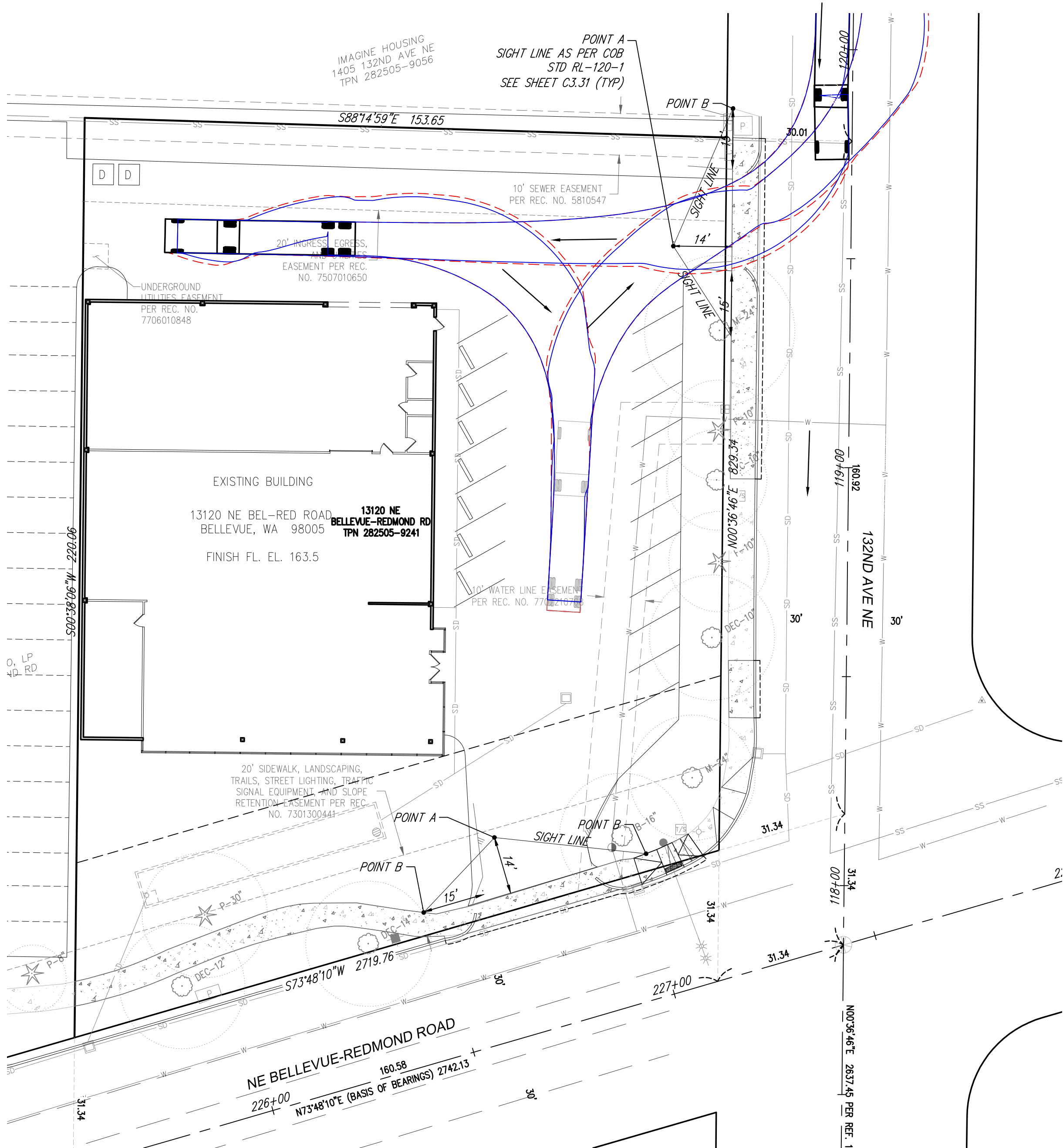
COB STD SW-210-1
SCALE: NONE



STORM DRAIN INLET (INSERT)
SCALE: NONE

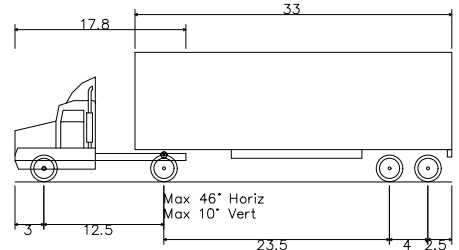


COB STD RL-120-1
SCALE: NONE

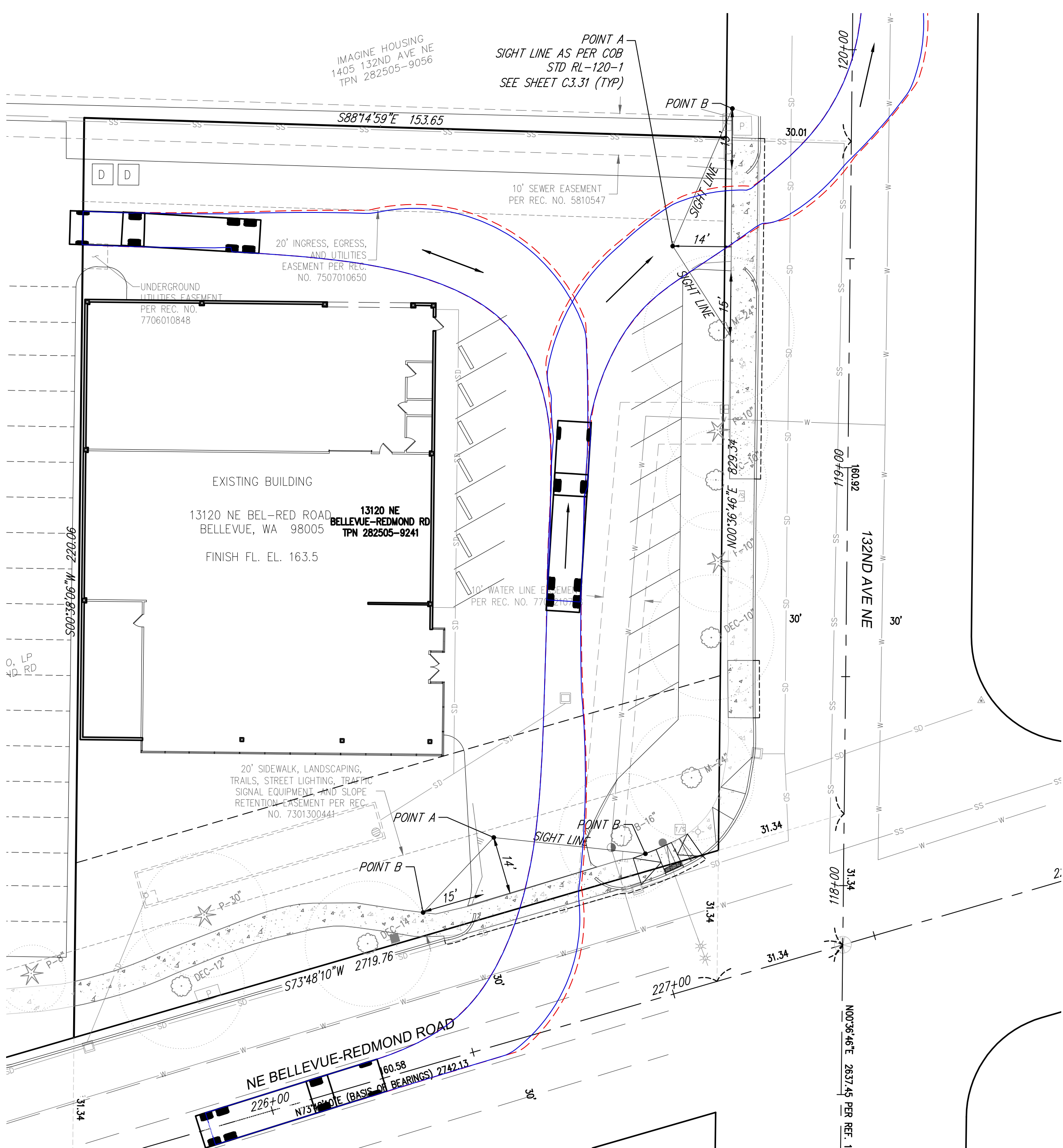


NOTES

1. AASHTO 2011, WB-40 VEHICLE WAS USED FOR THE SWEEP PATH ANALYSIS USING AUTODESKS VEHICLE TRACKING.
2. INGRESS SHOWN IS SOUTHBOUND ON 132ND AVE NE
3. EGRESS SHOWN IS NORTHBOUND ON 132ND AVE NE
4. LOADING AND UNLOADING IS IN THE REAR OF THE BUILDING (NORTH)



WB-40 - Intermediate Semi-Trailer
Overall Length 45.499ft
Overall Width 8.000ft
Overall Body Height 13.500ft
Min Body Ground Clearance 1.34ft
Track Width 8.000ft
Lock-to-lock time 4.00s
Max Steering Angle (Virtual) 20.30°

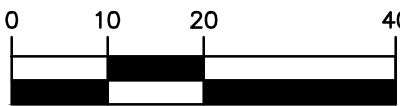


NOTES

1. AASHTO 2011, WB-40 VEHICLE WAS USED FOR THE SWEEP PATH ANALYSIS USING AUTODESKS VEHICLE TRACKING.
2. INGRESS SHOWN IS WESTBOUND ON NE BELLEVUE-REDMOND ROAD
3. EGRESS SHOWN IS NORTHBOUND ON 132ND AVE NE
4. LOADING AND UNLOADING IS IN THE REAR OF THE BUILDING (NORTH)



SCALE: 1" = 20'



UNDERGROUND LOCATOR SERVICE
CALL BEFORE YOU DIG!
1-800-424-5555

PERMIT NO. 20-XXXXXX GD
C.O.B. GRID #X & #X

DATE	DESIGNED	DRAWN	APPROVED	PROJECT MANAGER
JANUARY 2020	ANDREW OH E.I.T.	STEVE E. SIMMONS	MICHAEL A. MOODY, P.E.	MICHAEL A. MOODY, P.E.
DATE	DESIGNED	DRAWN	APPROVED	PROJECT MANAGER
2/19/20	ANDREW OH E.I.T.	STEVE E. SIMMONS	MICHAEL A. MOODY, P.E.	MICHAEL A. MOODY, P.E.
NO.	1	CITY COMMENTS		
REVISIONS				
DATE	2/19/20			

WB-40 SWEEP PATH ANALYSIS
13120 NE BEL-RED ROAD
AUTOMOBILI REAL ESTATE

11850 BEL RED RD
BELLEVUE, WA 98005

CIVIL ENGINEERING
LANDSCAPE ARCHITECTURE
PLANNING
SURVEYING

CORE DESIGN

12100 NE 195th St, Suite 300, Bothell, Washington 98011 425.885.7877

01-10-2020

PROJECT NUMBER
19207